

SLICK

Service Bulletin

SB1-06

TO: Aircraft manufacturers, distributors, dealers, engine overhaul facilities, owners and operators of SlickSTART™ ignition booster P/N SS1001.

SUBJECT: Removal of pin #2 on SS1001 SlickSTART™ ignition boosters.

BACKGROUND INFORMATION:

Previous installation instructions for the SS1001 allowed connection of the SlickSTART™ to both left and right magnetos. Unison Industries now recommends the removal of this connection method because it potentially compromises the redundancy of the ignition system. Therefore, this unit must be disconnected from the right hand magneto and pin #2 must be removed from the SlickSTART™ unit, thus allowing the right magneto to operate independently.

In most applications, this pin will not be connected, but the pin should still be physically removed to prevent any connection in the future.

COMPLIANCE: Within 20 flight hours, or at next scheduled maintenance, whichever is sooner, operators of aircraft that have the SS1001 SlickSTART™ booster installed should physically remove the #2 pin from the unit.

PROOF OF COMPLIANCE: Appropriate engine/airframe logbook entries.

TOOLS REQUIRED PER BULLETIN: Pin #2 can be cut off using standard side cutters. No other special tooling is required.

WEIGHT CHANGE: None.

MAGNETO MODELS AFFECTED: SlickSTART™ model SS1001.

DETAILED INSTRUCTIONS:

1. If installed, remove and discard the associated wire that connects the #2 pin to the P-lead on the right magneto.

WARNING: WEAR APPROPRIATE EYE PROTECTION WHEN CUTTING THE PIN.

2. If the SlickSTART™ unit is accessible, cut of the #2 pin as short as possible to prevent future use of this pin. See Figure 1. If necessary, remove the SS1001 from the firewall to complete this operation then re-install it.

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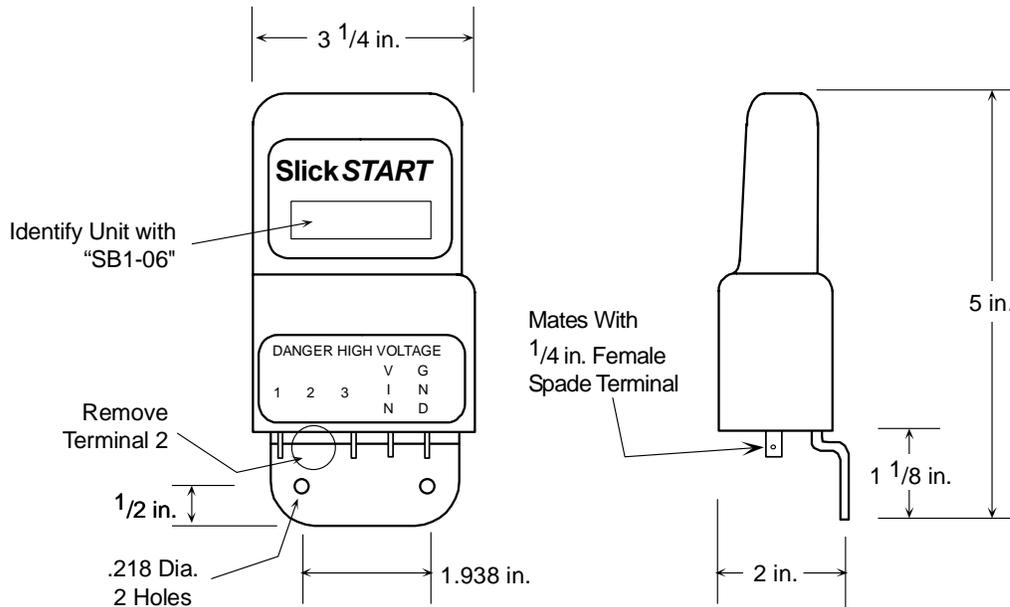


Figure 1

3. If the #2 pin was connected to the right magneto P-lead, an alternative method of grounding the right magneto P-lead during starting is provided below.

CAUTION: ON ENGINES WITH ONE RETARD BREAKER MAGNETO OR ONE IMPULSE COUPLING MAGNETO INSTALLED, FAILURE TO GROUND THE RIGHT MAGNETO DURING START WILL RESULT IN ENGINE KICKBACK AND POSSIBLE DAMAGE TO THE STARTER AND ASSOCIATED ACCESSORY GEAR TRAIN.

- A. In most applications, the right magneto can be grounded during start by the ignition switch. If the wiring connections on the back of the ignition switch are similar to those shown in Figure 2, a jumper can be added to the two terminals shown to provide a ground path during start.

Verify that this jumper is installed if your aircraft is equipped with this type of switch.

These diagrams show the most common switches. Refer to the switch manufacturer's installation instructions for your switch.

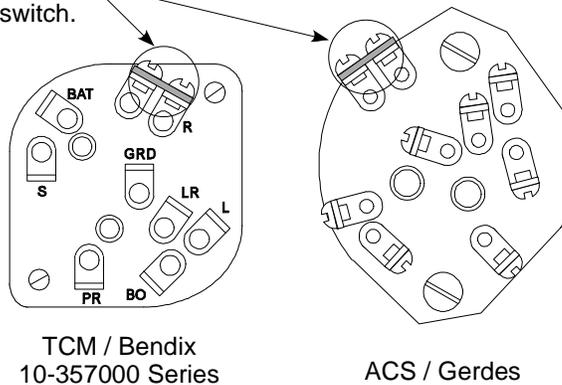


Figure 2

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- B. Some aircraft have individual magneto switches installed. In this circumstance, verify that the "pilot operating handbook" allows the right magneto switch to remain off during start, and placard the right magneto switch to remain off during start. If this method is not allowed by the aircraft manufacturer, a combination ignition / start switch shown must be installed to ground the right magneto during start.
- C. If any other type of start switch is installed that does not ground the right magneto during start, it must be replaced with a combination ignition / start switch that provides this function, or the SlickSTART™ booster must be removed from the aircraft.

PART IDENTIFICATION: Following removal of the #2 pin from the SlickSTART™, print using a permanent marker on the white portion of the unit's label "SB1-06". This marking will provide further indication that this Service Bulletin has been complied with.

THIS SERVICE BULLETIN IS A SUPPLEMENT TO SLICK MAINTENANCE AND OVERHAUL MANUAL L-1492. CHECK WITH SLICK TO BE SURE YOU HAVE THE MOST CURRENT REVISION OF SLICK MANUAL L-1492 AND PERTINENT SERVICE LETTERS AND BULLETINS BEFORE PERFORMING MAINTENANCE OR OVERHAUL.

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